

OPERATION THROUGH 13TH STREET INTERLOCKING

Normal operation through the 13th Street Interlocking Plant is automatic. The stub is used for emergencies such as to move a defective train off the main line or to turn a train back. It is also used to switch trains during single track moves. When it is used for these purposes, either a Towerman or a Supervisor operates the plant and a Supervisor is available to lend assistance.

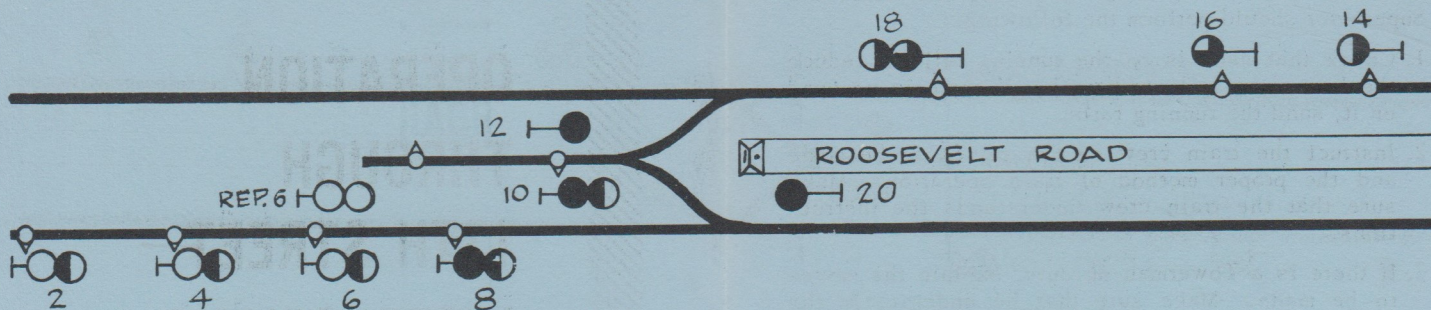
METROPOLITAN

TRANSIT



CHICAGO TRANSIT AUTHORITY

DIAGRAM OF TRACK AND SIGNALS



SIGNALS (see diagram)

All signals in the interlocking plant and in approach to it are color light signals. All signals except 12 and 20 have track trips.

NORTHBOUND SIGNALS

Signals 2, 4, and 6 are Interlocking Approach signals with allowable and closing speed features. The aspects and indications are as per Rule 146.

NOTE: Signal 6 has a repeater signal associated with it.

Signal 8 is an Interlocking Home signal with allowable speed feature. The aspects and indications are as per Rule 142.

Signal 12 is a Reverse Movement Interlocking Home signal. The aspects and indications are as per Rule 144.

SOUTHBOUND SIGNALS

Signals 14 and 16 are Interlocking Approach signals with closing speed feature. The aspects and indications are as per Rule 146.

Signal 18 is an Interlocking Home signal. The aspects and indications are as per Rule 142. This signal displays the yellow over yellow over yellow aspect for trains operating to the stub. The indication for this signal is proceed with caution at restricted speed.

Signal 20 is a Reverse Movement Interlocking Home signal. The aspects and indications are as per Rule 144.

SIGNAL IN THE STUB

Signal 10, an Interlocking Home signal, is the only signal in the stub. The aspects and indications are as per Rule 142.

CAUTION: If a train is tripped within the limits of an Interlocking Plant, it is the responsibility of the person operating the train to alight and inspect the full length of the train to determine the cause before proceeding.

If the cause was an interlocking home signal trip, Rule 155 must be followed.

If the cause cannot be determined, the person operating the train must call the Line Supervisor before operating "on sight."

TRAIN MOVEMENT WITHIN THE STUB

Whenever trains are operated within the stub, the person operating the train must:

1. Operate at restricted speed not to exceed 6 MPH.
2. Use the track brake to assist braking.

CAUTION: Because the stub is seldom used, the rail is extremely slippery and trains must be operated cautiously to prevent sliding. Sand is to be used as necessary to avoid sliding.

3. When operating an 8-car train into the stub, bring the front car all the way to the bumping block, first making a safety stop 5 feet in front of the bumping block.
4. When operating a train out of the stub against traffic, operate the manual track trip release when the call-on aspect is displayed.
5. Check the switch position before moving the train.

SUPERVISOR'S RESPONSIBILITIES

Whenever moves are to be made in the stub, the Supervisor should perform the following:

1. Check that sand is on the running rails to reduce the hazardous rail conditions. If there is no sand on it, sand the running rails.
2. Instruct the train crew on the moves to be made and the proper method of train operation. Make sure that the train crew understands the instructions.
3. If there is a Towerman on duty, explain the moves to be made. Make sure that he understands the moves.
4. Do not route a train out of the stub if a train is approaching the limits of the interlocking plant on the track to which the stub train is to be routed.
5. Route trains out of the stub immediately after a road train passes.
6. Inform the Line Supervisor of all emergency moves made at the interlocking plant.